

Fehr & Peers

Memo

Date: February 26, 2026
To: Mykal Tyson, The Nest on 17th Company
From: Paul Herrmann, P.E.
Brian Wolfe
Azriel Olmedo

Subject: City of Costa Mesa The Nest on 17th Event Space Trip Generation Assessment

OC25-1161

Fehr & Peers reviewed The Nest on 17th event space project and determined that the project does not warrant further traffic analysis based on the size and hours of operation of the project. A detailed description of the project and project trip generation estimates are provided below.

Project Description

The project is located at 932 W 17th Street in Costa Mesa, California. The site is zoned as MG – General Industrial and has a General Plan land use designation of Light Industrial. The existing building has been occupied by The Nest at 17th Company operating approximately 4,200 square feet of gross floor area. The site includes 22 permanent parking spaces: 14 standard parking spaces, six compact parking spaces, and two ADA parking spaces.

The proposed project would establish a new event space primarily used for baby showers and birthdays with a typical range of 40 to 75 attendees, and three employees. Larger events such as weddings and corporate events are expected to be infrequent, occurring approximately twice per month. These large events are expected to be typically 120 guests with a maximum occupancy for any event of 150 guests along with five employees and five additional vendors. The Nest at 17th would not allow multiple events concurrently or within the same day.

The event space hours of operation are:

- 4 PM to 10 PM on Fridays
- 12 PM to 10 PM on Saturdays
- 12 PM to 8 PM on Sundays

All event preparation occurs on the day of the event. Administrative functions during non-event hours will be handled by five personnel. The event site will also see limited staff-only use between 10 AM to 4 PM on Tuesdays and Thursdays.

Trip Generation

Project trip generation estimates are typically prepared for time periods where adjacent street traffic is highest, which for this project are mid-week (Tuesday–Thursday) peak commute hours (7 AM – 9 AM and 4 PM – 6 PM). However, due to the event space hours of operation, the project is not anticipated to generate most of its traffic during peak time periods.

Friday night events are planned to operate between 4 PM and 10 PM. Therefore, the peak hour of arrival is assumed to be 4 PM to 5 PM while the peak hour of departure is assumed to be 9 PM to 10 PM. Typically, project trip generation estimates are determined using trip generation rates from the Trip Generation 12th Edition ITE Handbook; however, the handbook does not provide rates for events that are proposed as part of this project. Therefore, the following assumptions were applied to estimate arrival/departure trip generation:

- An average vehicle occupancy (AVO) of two people per vehicle for attendees, and an AVO of one person per vehicle for employees and vendors
- 15% of attendees utilize rideshare/taxi services (counted as one inbound trip and one outbound trip)
- Employees/event staff arrive/depart outside the guest arrival/departure peak hour
- Peak hour of arrival is 4 PM to 5 PM and peak hour of departure is 9 PM to 10 PM
 - 100% of employees arrive before start of event (3–4 PM)
 - 90% of attendees arrive at start of event (4–5 PM)
 - 10% of attendees arrive before or after start of event (3–4 PM or 5–6 PM)
 - 10% of attendees depart just before end of event (8–9 PM)
 - 90% of attendees depart at end of event (9–10 PM)
 - 100% of employees depart after the end of the event (10–11 PM)

Table 1 shows the Project daily and peak hour trip generation estimates under a range of scenarios. Note that for weekend events, the peak hour of arrival and departure may differ depending on event start and end times.

Table 1. Proposed Land Use Trip Generation Estimates

Scenario and Attendee Type		Attendees	AVO	Total Daily Trips	Peak Hour Arrival		Peak Hour Departure	
					4–5 PM		9–10 PM	
					In	Out	In	Out
Small Events (Showers, Birthdays)	Attendees (Self Drive) (85%)	34	2	34	15	0	0	15
	Attendees (Rideshare) (15%)	6	2	12	3	3	3	3
	Vendors/Employees	10	1	20	0	0	0	0
	Total	50		66	18	3	3	18
Medium Events (Showers, Birthdays, Weddings)	Attendees (Self Drive) (85%)	64	2	64	29	0	0	29
	Attendees (Rideshare) (15%)	11	2	22	5	5	5	5
	Vendors/Employees	10	1	20	0	0	0	0
	Total	85		106	34	5	5	34
Large Events	Attendees (Self Drive) (85%)	102	2	102	46	0	0	46
	Attendees (Rideshare) (15%)	18	2	36	8	8	8	8

(Weddings)	Vendors/Employees	10	1	20	0	0	0	0
	Total	130		158	54	8	8	54
Maximum Capacity	Attendees (Self Drive) (85%)	128	2	128	58	0	0	58
	Attendees (Rideshare) (15%)	22	2	44	10	10	10	10
	Vendors/Employees	10	1	20	0	0	0	0
	Total	160		192	68	10	10	68

Source: Fehr & Peers, 2025

The above table estimates that the typical small and medium events will generate up to 21 and 39 total peak hour trips, respectively. Higher capacity events are likely to generate up to 78 trips during the arrival and departure period immediately prior to or following the event. This is anticipated to occur on the weekends in the late evening when adjacent street traffic volumes are lower. If a larger event (150 attendee capacity) occurs on Friday evening (at most twice per month), there may be as many 78 trips accessing the site overlapping with the evening commute period.

Per the *City of Costa Mesa Transportation Impact Analysis (TIA) Guidelines* (2020), a transportation impact study shall be required for all development projects estimated to regularly generate one hundred (100) or more vehicle trip ends during a peak hour. **As the project primarily operates outside of AM and PM weekday peak hours, it will not generate enough vehicle trips to warrant further study.**

Parking Assessment

Fehr & Peers also reviewed available off-street parking supply to ensure parking needs are met. The Project provides up to 47 off-street parking spaces for use by employees, vendors, and attendees:

- **22 permanent spaces:** 14 standard parking spaces, six compact parking spaces, two ADA parking spaces
- **25 valet spaces** when valet service is provided

The number of off-street spaces is sufficient to accommodate the expected 37 vehicles for medium-sized events (32 attendee vehicles plus five employee/vendor vehicles as needed).

For larger and maximum capacity events that occur approximately twice per month, the total parking demand is estimated to be 61 vehicles and 74 vehicles, respectively, ten of which will be for employees and vendors. Consistent with AB 2097 (Government Code Section 65863.2(d)), an event center within a transit priority area shall provide parking for employees and other workers. The proposed valet parking plan will accommodate all employee/vendor parking needs and most visitors.

It should also be noted that there is ample on-street parking available during off-peak and weekend hours along W 17th Street (see attached field observation photos). While on-street parking supply is not required to accommodate typical operations, these spaces can be used by visitors and vendors as needed.

Attachment: Parking Plan

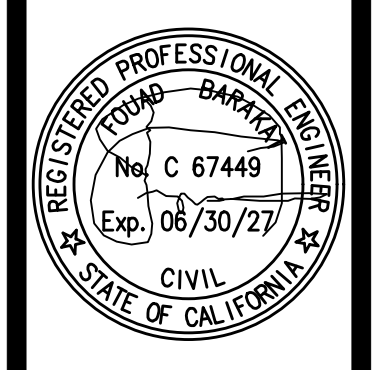
Photos of on-street parking on West 17th Street – Saturday at 4:15pm



REVISIONS	
NO.	DATE
1	DATE
2	DATE
3	DATE

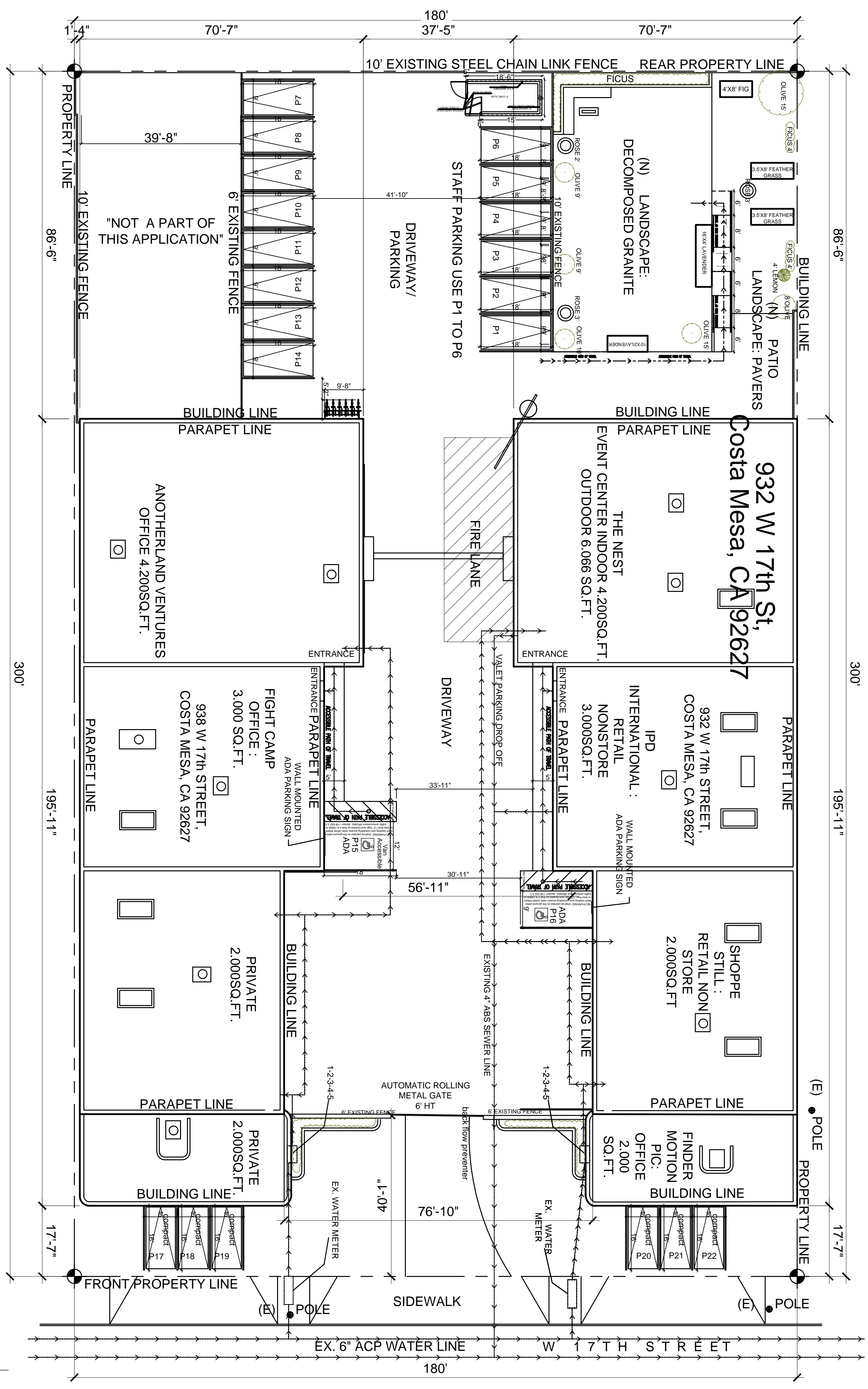
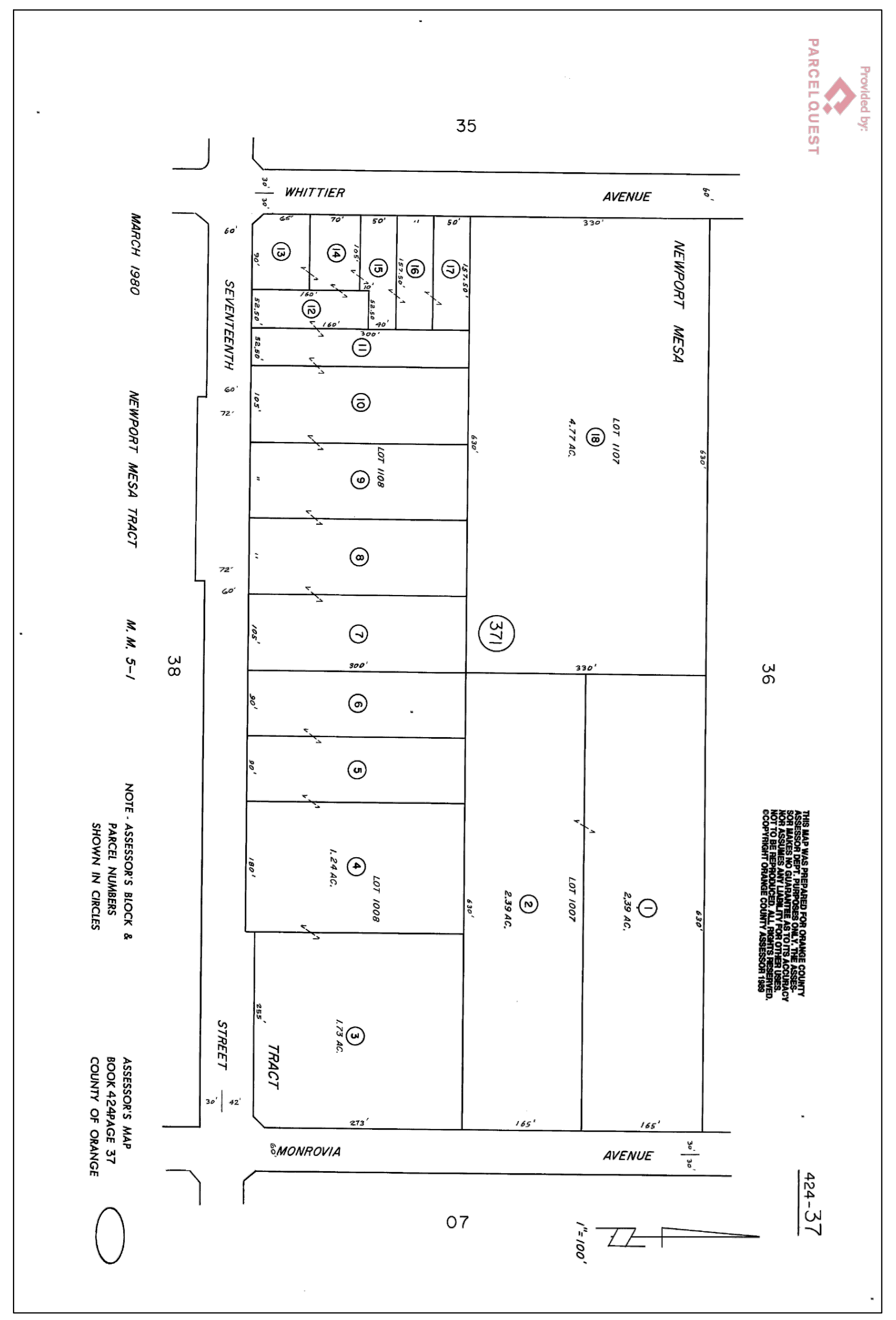
ALL CITIES CONSULTING ENGINEERS
 8615 FLORENCE AVE #102
 DOWNEY, CA 90240
 TEL.: (562) 861-7900

932 W 17th St, Costa Mesa, CA 92627



DATE: JUNE 2025
 DRAWN BY: A.C.C.E.
 SCALE: AS NOTED
 TITLE: PROPOSED SITE PLAN

SHEET: SP3



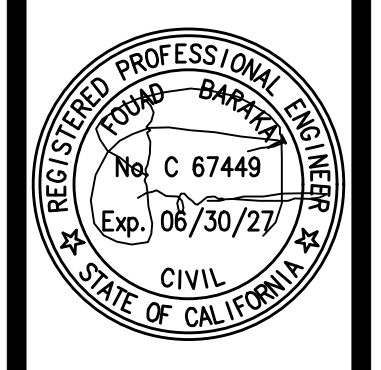
PROPOSED SITE PLAN
 SCALE: 1/16"=1'

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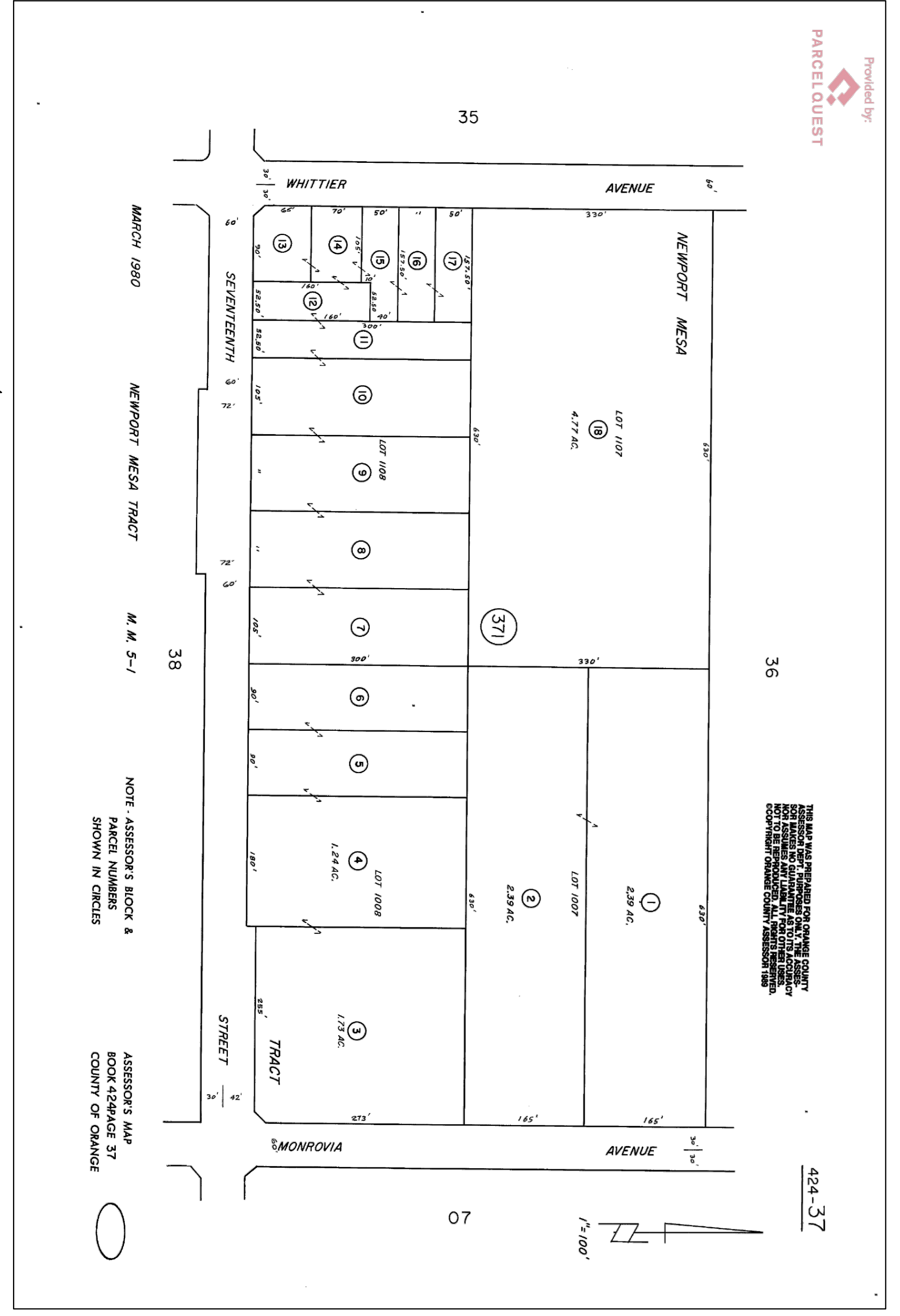
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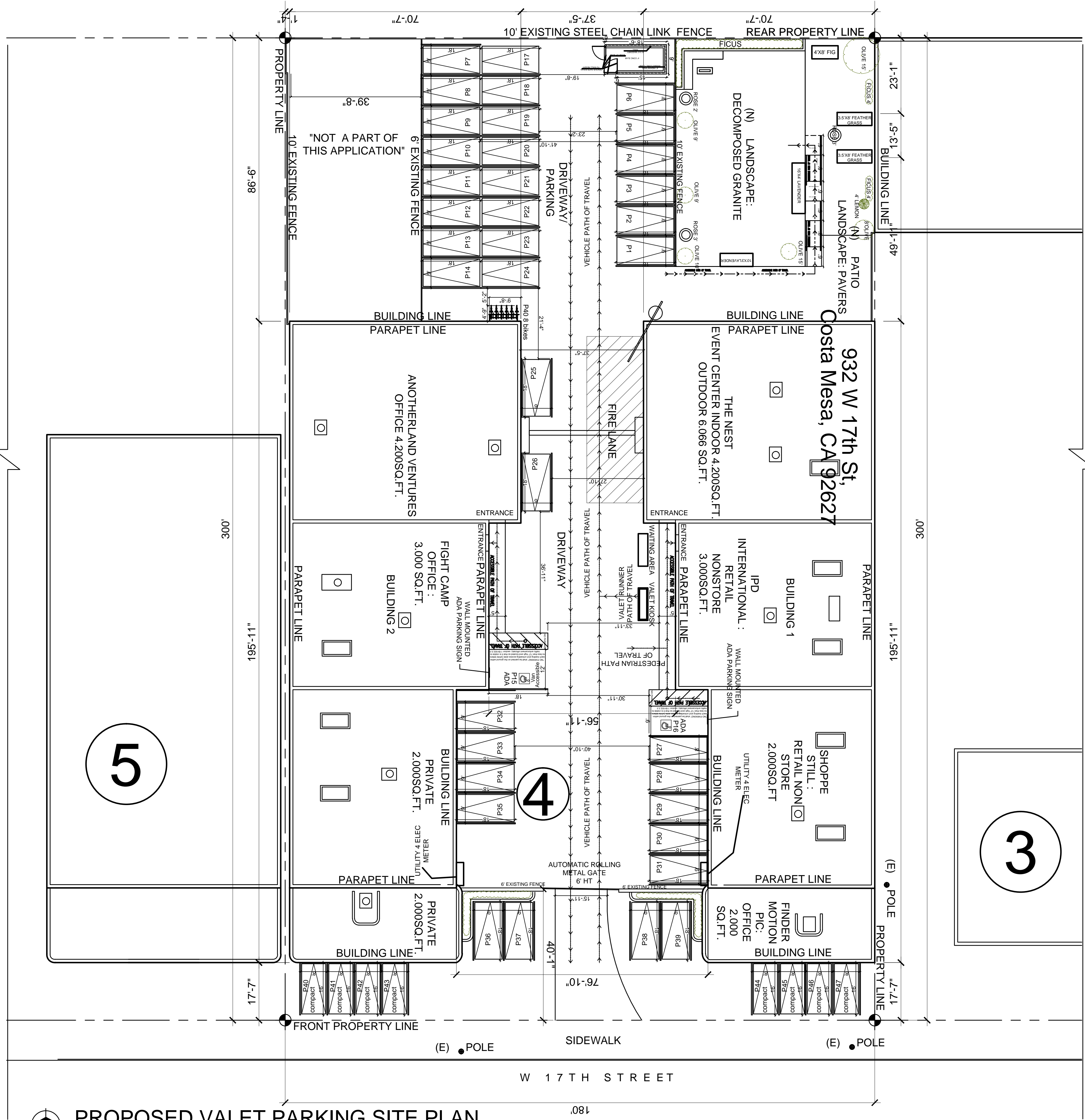


DATE: JUNE 2025
 DRAWN BY: A.C.C.E.
 SCALE: AS NOTED
 TITLE: VALET PLAN
 SHEET: SP4



NOTE - ASSESSOR'S BLOCK & PARCEL NUMBERS SHOWN IN CIRCLES

ASSESSOR'S MAP BOOK 429 PAGE 37 COUNTY OF ORANGE



PROPOSED VALET PARKING SITE PLAN
 SCALE: 1/16"=1'

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