SUBJECT: DESIGN REVIEW (PA-23-14) AND TENTATIVE PARCEL MAP 2023-

117 REQUEST FOR A RESIDENTIAL SMALL LOT SUBDIVISION TO CREATE TWO PARCELS, EACH CONTAINING A NEW TWO-STORY DETACHED SINGLE-FAMILY RESIDENCE AND AN ATTACHED TWO-

CAR GARAGE AT 2308 SANTA ANA AVENUE

FROM: ECONOMIC AND DEVELOPMENT SERVICES

DEPARTMENT/PLANNING DIVISION

PRESENTATION BY: CAITLYN CURLEY, ASSISTANT PLANNER

FOR FURTHER CAITLYN CURLEY INFORMATION 714-754-5692

CONTACT: Caitlyn.Curley@costamesaca.gov

RECOMMENDATION:

Staff recommends the Planning Commission adopt a Resolution to:

- Find that the staff presentation is exempt from the provisions of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15) Minor Divisions of Land, and Section 15303 (Class 3) New Construction or Conversion of Small Structures. In addition, this project is statutorily exempt from the requirements of CEQA pursuant to Public Resources Code section 21080.66; and
- 2. Approve Parcel Map No. 2023-117 and Design Review PA-23-14 based on findings of fact and subject to conditions of approval.

APPLICANT OR AUTHORIZED AGENT:

The applicant and authorized agent is the property owner, Ali Sedghi of Eastside Coastal LLC.

PLANNING APPLICATION SUMMARY

Location:	2308 Santa Ana Avenue	Application Number:	PA-23-14
Request:	Parcel Map and Design Review for a re	sidential small lot subdivi	sion project consisting
	of two, two-story, detached single famil	ly dwelling units with attac	hed two-car garages.

SUBJECT PROPERTY:

SURROUNDING PROPERTY:

70202011 NOT 20011 NOT 200				
Zone:	R2-MD (Multi-Family	North:	R2-MD	
	Residential, Medium Density)			
General Plan:	Medium Density Residential	South:	R2-MD	
Lot Dimensions:	63' x 150'	East:	R2-MD	
Lot Area:	9,450 SF	West (across Santa Ana Ave):	R1	
Existing	One single-story, single-family dwelling with a detached, nonconforming two-car			
Development:	garage.			

DEVELOPMENT STANDARDS COMPARISON

Development	Small Lot Standards	Proposed/Provided	Code Compliant?	
Standard				
Lot Size		5 14 44 57 4 5 19 1		
Lot Width/Depth	No Standard	Parcel 1: 44 FT 1.5 IN /	Yes	
		82 FT		
		Parcel 2: 63 FT / 150 FT		
Lot Area	3,630 SF	Parcel 1: 3,862 SF	Yes	
		Parcel 2: 5,588 SF		
Density/Intensity				
DUs Per Acre	1 DU / 3,630 SF of Lot	Total of two units	Yes	
(Residential)	Area			
	2 units maximum			
	allowed			
Building Height				
	2 Stories / 27 FT	2 stories / 26 FT 5 IN	Yes	
Open Space				
Overall Open Space	35% of development	4,333 SF (46%)	Yes	
	area			
Private Open Space	200 SF / Min. 10 FT	Parcel 1: 1,213 SF	Yes	
		Parcel 2: 1,598 SF		
Residential Guidelines		,		
2 nd Floor to 1 st Floor	Maximum 100%	Parcel 1: 98.5%	Yes	
Ratio		Parcel 2: 86.4%		
Development Lot Building	Setbacks		'	
Front	20 FT	20 FT 1 IN	Yes	
Side (left)	5 FT	5 FT 1 IN	Yes	
Side (right	5 FT	5 FT 1 IN	Yes	
Rear	15 FT	15 FT 1 IN	Yes	
Distance Between	6 FT	30 FT 4 IN	Yes	
Main Buildings				
Parking				
Garage	2-car garage per unit	2-car garage/unit (4	Yes	
		spaces)		
Open	2 spaces per unit	2 spaces/unit (4 spaces)	Yes	

Total Parking		8 spaces	8 spaces	Yes
CEQA Review	Exempt per CEQA Guidelines Section 15315 (Minor Division of Land) and Section 15303 (New Construction or Conversion of Small Structures)			f Land) and Section
Final Action	· · · · · · · · · · · · · · · · · · ·	g Commission		

EXECUTIVE SUMMARY

The applicant is requesting Planning Commission approval of a small two-lot residential subdivision project for the development of two new parcels, each with a two-story, detached single-family residence and an attached two-car garage. The proposed subdivision would allow for individual ownership of the units. Staff supports the request because the proposed small lot subdivision project is consistent with applicable goals, objectives, and policies of the General Plan, complies with applicable provisions of the Zoning Ordinance and respective findings, and would be compatible with surrounding residential development.

BACKGROUND

The project site is located on the northeastern side of Santa Ana Avenue, near the intersection of Santa Ana Avenue and 23rd Street, in eastside Costa Mesa. The project site is an interior lot located on a level, rectangularly shaped 9,450 square foot parcel.

The property has a General Plan Land Use designation of Medium Density Residential (MDR) and is zoned Multi-Family Residential District, Medium Density (R2-MD). The project site is surrounded by residential development that consists of multi- and single-family residences. Properties abutting the project site to the north, east and south are designated under the R2-MD zone. The properties to the west, across Santa Ana Avenue, are designated under the R1 zone.

Current development on the property consists of one vacant single-family residence constructed between 1938 and 1947, based on historic aerial photographs. There is no permit on file with the City for the construction of the residence due to the age of the building. The residence has a driveway with access from Santa Ana Avenue that leads to a two-car garage. The residence has been owner occupied for more than five years and it therefore not a protected unit. This structure would be demolished to allow for the proposed project.

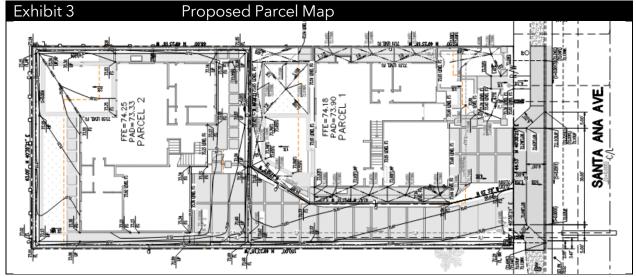




REQUEST

The applicant is requesting approval for a Small Lot Subdivision for the development of two single family residences and attached garages. Small Lot Subdivisions are allowed in the R2-MD zoning district pursuant to Costa Mesa Municipal Code (CMMC) Section 13-42, with the approval of a Tentative Parcel Map and Design Review applications. This project does not qualify for a regular subdivision because the resultant lots and structures do not meet the standards required for subdivisions and new construction in the R2-MD zone without the allowances of the Small Lot Subdivision requirements.

The proposed development, as shown in Exhibit 3 below, will subdivide the 9,540-square-foot parcel into two parcels. Parcel 1 is proposed to be 3,862 square feet and Parcel 2 is proposed to be 5,588 square feet. Each parcel proposes a two-story single-family residence with an attached two-car garage that will be oriented towards Santa Ana Avenue.



ANALYSIS

Residential Small Lot Subdivision / Tentative Parcel Map

The CMMC requires that the maximum density of a small lot subdivision be consistent with the underlying zoning district (R2-MD, Multiple-Family Residential District, Medium Density) and required development standards are specified pursuant to CMMC Section 13-42.3(b)(1-6). Parcels greater than 7,260 square feet in area are allowed a density of 1 unit per 3,630 square feet of lot area. The project proposes two parcels each with a minimum lot area greater than 3,630 square feet and therefore complies with the density requirements. A comparison of the required development standards and the proposed project is provided above in the "Development Standards Comparison" table. As indicated in this table, the project is designed in compliance with the City's standards.

Additionally, the project has been reviewed by the Building Division and conditions of approval have been included to ensure compliance with the California Building Code.

The existing 9,540 square-foot property is rectangularly shaped and is 150 feet deep and 63 feet wide. The Tentative Parcel Map application proposes the creation of two lots consisting of Parcel 1 (3,862 square feet) and Parcel 2 (5,588 square feet). Parcel 1 is proposed as rectangular shaped abutting Santa Ana Avenue with 44 feet 1.5 inches of street frontage, and Parcel 2 is proposed to be located mostly behind Parcel 1 with the front property line extending to Santa Ana Avenue for driveway access. Parcel 2 has a street frontage of 18 feet 10.5 inches on Santa Ana Avenue. The City's Residential Small Lot Subdivision standards do not require a minimum lot size.

Design Review

The proposed development includes two new two-story single-family residences that are approximately 2,646 square feet (Unit 1) and 3,060 square feet (Unit 2), with attached two car garages, and four bedrooms. The maximum height of the residences is proposed to be 26 feet 5 inches.

Pursuant to CMMC Section 13-42.2, a new residential small lot development project shall be processed according to the City's Design Review procedures (CMMC Section 13-29). The City's Design Review application process ensures that the proposed development complies with the Costa Mesa Zoning Code and meets the intent of the *Residential Design Guidelines*. Table 1 below provides a summary of the residential development proposed on each Parcel.

Table 1	Unit Characteristics							
	1 st	Garage	2 nd Floor	2 nd	Bed-	Bath-	Parking	Lot Area
	Floor		Area	Floor	room	rooms	Spaces	
	Area		(includin	/ 1 st	Count	(Full/Half)	(Garage	
			g	Floor			/Open)	
			balcony)	Ratio				
Unit 1	1,118	434 SF	1,528 SF	0.98	4	4 Full	2/2	3,862 SF
	SF			5				
Unit 2	1,444	430 SF	1,618 SF	0.86	4	4 Full/	2/2	5,588SF
	2 SF			4		1 Half		

Residential Design Guidelines

The proposed development, as shown in Exhibit 4 below, meets the intent of the City's Residential Design Guidelines as follows:

- Second-Story Design and Building Mass: The City's Residential Design Guidelines recommend second-story floor areas (including balconies) to not exceed 100 percent of the first-story floor area (including attached garage areas). The second-story floor area of Unit 1 has been designed with an area of 98.5% of the first-story floor area and Unit 2 has been designed with a floor area that is 86.4% of the first-story floor area. Each proposed residence has been designed with articulation and off-sets on the various elevations to avoid boxy appearances. The elevations for the proposed development include multiple building planes and varied roof forms to soften the mass of the second story. The elevations also include varied facades and articulations with brick veneer and horizontal wood siding and stucco.
- **Second-Story Setbacks**: According to the *Residential Design Guidelines*, "the second-story interior side building elevation should be set back an average of 10 feet but shall be no closer than five feet from the side property line". However, the *Guidelines* specify that only a five-foot second-story setback is required for units less than 2,700 square feet of living area. Unit 1 is proposed at 2,646 square feet and is therefore exempt from the second-story setback requirements. Unit 2 has a floor area of 3,060 square feet and has been designed with 10-foot average side setbacks to meet this standard.
- **Elevation Treatments**: All units feature a variety of projections and include articulation on each wall, varying roof types and materials. Building materials include brick veneer and horizontal wood siding, white stucco and composite shingle roofing.
- Window Placement: Consideration for design and privacy has been provided in locating the windows on the property. The applicant provided a "line of sight study" which demonstrates that the second-story windows facing north, and south are located without direct views into the neighboring structures. Windows with direct views into usable side and rear yards provide privacy with either glazing or by utilizing a five-foot sill height. One window in Bedroom 2 of Unit 1 is required to meet egress requirements for the California Building Code. In this case, the window has been designed to meet the minimum size required by the Building Code to meet egress requirements in order to mitigate privacy impacts to the neighboring backyard. The first-story windows are located behind the proposed six-foot block wall. The block wall is proposed to be constructed on the property line between the neighboring properties, which would further eliminate privacy impacts for the existing neighboring properties.
- **Consistency in Architectural Design**: The proposed design includes California Coastal Modern architecture with materials and finishes that remain durable and require minimal maintenance. In addition, each façade features a variety of

overhangs, materials, and roof forms, which provides visual interest and façade articulation (see the below Exhibit 4).



The immediately surrounding neighborhood features a diverse collection of architectural styles including other residences with modern and eclectic influences, and residences with craftsman features, eclectic styles and traditional styles (see the below Exhibit 5). The varying use of materials ranging from stucco and brick veneer to wood siding and the use of articulation and off-sets for two-story residences are common throughout the neighborhood which would be compatible with the proposed design.



Open Space

The CMMC requires that each unit be provided a minimum of 200 square feet of open space with no dimension being smaller than 10 feet. The units are proposed to be constructed to comply with the CMMC required private open space. Unit 1 provides 1,213 square feet of private open space with a minimum of 10 feet in depth and Unit 2 provides 1,598 square feet of private open space with a minimum of 10 feet in depth. Pursuant to the CMMC, each unit will also provide adequate space outside of each unit for three trash containers without encroaching into the garage space and without being visible from the public right of way.

Parking and Circulation

Pursuant to the City's Small Lot Subdivision Standards and as provided by the project, units proposed with four bedrooms require two garage and two open parking spaces for each parcel (see Table 2 below).

Table 2 Parking Requ	ng Requirements			
	Requirement	Proposed		
Garage Parking Spaces	4 spaces (2 per unit)	4 spaces		
Open Parking	4 spaces (2 per unit for 3 or	4 spaces		
	more bedroom units)			

Pursuant to the Residential Small Lot Subdivision standards, driveway width is required to be 10 feet. The project complies with a 17-foot-wide driveway on Parcel 1 and a 10-foot-wide driveway on Parcel 2. Each driveway provides access to the respective unit's two-car garage and two open-parking spaces located on each driveway. Both the City's Fire Department and Public Works Department have reviewed the project access design for conformance with minimum safety and access criteria. As required by the CMMC and as conditioned, all parking and driveways shall consist of decorative concrete, pavers, or other material subject to review by the Development Services Director.

Fences and Walls

Pursuant to CMMC Section 13-75, the project requires the installation of a six-foot tall masonry wall around the perimeter of the proposed development lot. The final design of the walls will be reviewed as part of the building permit submittal. Any future modifications to the site walls and fencing are subject to review and approval of the Planning Division and may require building permits to be issued prior to installation. The heights and locations of walls and fences shall comply with the CMMC requirements as well as applicable visibility standards for traffic safety.

Landscaping and Lighting

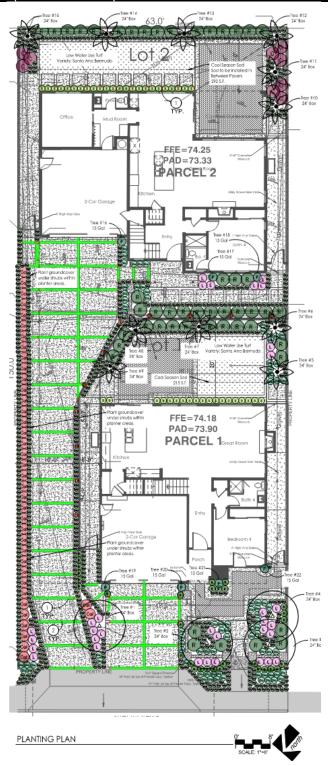
Per CMMC Section 13-106, all required landscape areas shall consist of drought tolerant plant material and shall meet the minimum number of plant types based on the total landscape square footage. The project proposes a total of 2,428 square feet of landscaping with a mixture of usable and decorative landscaping. The number of plants and trees required and proposed is provided in Table 3 below.

Table 3 Landscaping Requirements				
	Requirement	Proposed		
Tree Count	15 (one, 15-gallon tree or larger per 200	16.5 ¹		
	square feet of landscape area)			
Shrub Count	305 (one shrub for every 25 square feet of	305		
	landscape area)			
Groundcover	70% with the remaining area to incorporate	70%		
coverage	uncontaminated compost/mulch			
1 1 1	44 			

¹ The project proposes 11 palm trees, which each count as one-half of a tree for the purposes of meeting the Landscaping Standards.

As part of the building permit plan check review, landscape plans will be prepared and certified by a California licensed landscape architect confirming compliance with the CMMC and water efficiency landscape guidelines (see Exhibit 6 which shows the proposed landscape plans for Parcels 1 and Parcel 2).

Exhibit 6 Conceptual Landscape Plans



Utilities

The CMMC requires that new construction provide undergrounding of all utilities on site, including existing utility poles. As required, all new and existing utilities will be installed underground. Conditions of approval require that any new backflow preventers or related equipment be installed outside of the front landscape setback and be screened from view from any location on- or off-site. As required by the CMMC, all utility meters shall be screened from view from the public right of way and neighboring properties. Conditions of approval will require that prior to building permit approval, the applicant shall submit for approval of a comprehensive utilities plan to ensure that the water and sewer mains are adequate, and utility upgrades will be required if the existing infrastructure is not adequate. The plan will be reviewed by both the City's Building Division and Public Works Department at time of building permit submittal.

GENERAL PLAN CONFORMANCE

The following analysis further evaluates the proposed project's consistency with specific policies and objectives of the 2015-2035 General Plan.

1. **Policy LU-1.3**: "Strongly encourage the development of residential uses and owner-occupied housing (single-family detached residences, condominiums, townhouses) where feasible to improve the balance between rental and ownership housing opportunities."

Consistency: The proposed project is in conformance with General Plan Policy LU-1.3 in that the proposed scope of work consists of demolishing one single-family dwelling and one detached garage located on a multifamily zoned parcel (R2-MD) and to construct two, two-story detached ownership single-family dwelling units with attached garages on separate parcels, which will maximize the ownership opportunities of a residential property.

2. **Objective LU-2A**: "Promote land use patterns and development that contribute to community and neighborhood identity."

Consistency: The project site is surrounded by single- and multi-family zoned parcels featuring various types of single and multi-family development. The proposed project complies with General Plan Objective LU-2A in that the proposed multi-unit development is consistent with the types of development within the surrounding neighborhood and contributes to architectural design that complies with the residential design standards in the neighborhood.

3. **Policy HOU-3.4**: "Consider the potential impact of new housing opportunities and their impacts on existing residential neighborhoods when reviewing development applications affecting residential properties."

Consistency: The project would not negatively affect surrounding residential properties as privacy impacts have been considered in the development of the project and the proposed development will generally improve neighborhood and streetscape aesthetics. The project would be located in an established residential neighborhood and would increase the total opportunities for individual housing ownership in the City.

FINDINGS

Pursuant to Title 13, Section 13-29(g)(13) and (14), "Findings for Tentative Parcel Maps and Design Review," to approve the project, the Planning Commission must find that the evidence presented in the administrative record substantially meets the following applicable required findings:

Tentative Parcel Map Findings - Costa Mesa Municipal Code Section 13-29(g)(13)

The creation of the subdivision and related improvements is consistent with the General Plan, any applicable specific plan, and the Zoning Code.

The Medium Density Residential General Plan land use designation establishes a maximum allowable density of 12 units per acre. The subject parcel is 9,450 square feet in area. The parcel map would allow for a new residential project that would be consistent with the General Plan land use designation of Medium Density Residential and the R2-MD zone that allows for one unit per 3,630 square feet of lot area. The project design also complies with the applicable development standards for a residential small lot subdivision. The proposed parcel map is consistent with General Plan Land Use Objectives LU-1A and LU-2A, in that the subdivision allows for a project that would promote homeownership opportunities and improve the balance between rental and ownership housing in the City, and the parcel map would allow for redevelopment of an existing parcel which would improve and maintain quality of the neighborhood by improved architecture, aesthetics, and landscaping.

The proposed use of the subdivision is compatible with the General Plan.

The subject property has a General Plan land use designation of Medium Density Residential, which allows multi-family residential uses at a maximum of 12 dwelling units per acre. The subject parcel is 9,450 square feet in area. The Parcel Map proposes a residential use that would not exceed the maximum density allowed per

the General Plan. In addition, the subdivision allows for a project that would promote homeownership opportunities and improve the balance between rental and ownership housing in the City, and the parcel map would allow for redevelopment of an existing parcel which would improve and maintain quality of the neighborhood by improved architecture, aesthetics, and landscaping, pursuant to General Plan Land Use Objectives LU-1A and LU-2A.

The subject property is physically suitable to accommodate the subdivision in terms of type, design and density of development, and will not result in substantial environmental damage nor public health problems, based on compliance with the Zoning Code and General Plan, and consideration of appropriate environmental information.

The property is relatively flat regarding topography, located within an established residential neighborhood, currently improved with residential development and suitable to accommodate two residential units. The size of the lot is also suitable since the proposed development meets all applicable development standards including, setbacks, parking, and open space. The parcel map proposes to subdivide a parcel into two lots that does not exceed the minimum R2-MD maximum density of one dwelling unit for 3,630 square-feet of lot area. Adequate infrastructure exists to serve the proposed project and the project will not result in the loss of any habitat, result in a negative impact on the environment as a whole or require extensive infrastructure improvements to provide service to the site.

The design of the subdivision provides, to the extent feasible, for future passive or natural heating and cooling opportunities in the subdivision, as required by State Government Code section 66473.1.

The project provides 45.8 percent open space for the overall development and each unit will have at least 200 square feet of private open space area. The open space will accommodate landscaping that can be provided throughout the site with adequate setbacks for airflow, and trees for site shading. The project is proposed to include operable windows and will be fully insulated as required by the building code.

The division and development will not unreasonably interfere with the free and complete exercise of a public entity and/or public utility rights-of-way and/or easements within the tract.

The project has been reviewed by the Public Works Department and there are no conflicts with the City's or other utility agencies' rights-of-way or easements.

The discharge of sewage from this land division into the public sewer system will not violate the requirements of the State Regional Water Quality Control Board pursuant to Division 7 (commencing with State Water Code section 13000).

The lot currently has connections to the public sewer system for the existing residential unit. The parcel map would allow for a residential project that would not include physical changes to the lot that would result in discharge into the public sewer system in violation of State requirements. Furthermore, the applicant will be required to comply with the regulations set forth by the Costa Mesa Sanitation District and Mesa Water District. Compliance with the Costa Mesa Sanitation District and Mesa Water District involves the implementation of a Stormwater Pollution Prevention Plan (SWPPP) for construction-related activities, which will specify the "best management practices" (BMPs) that the project will be required to implement during construction activities to ensure that all potential pollutants of concern (including sediment) are prevented, minimized, and/or otherwise appropriately treated prior to being discharged from the subject property.

As indicated above, the project complies with above findings to approve the Tentative Parcel Map. In addition, pursuant to the Subdivision Map Act, Government Code Section 66474, a parcel map must be denied if one or more findings are made:

That the proposed map is not consistent with applicable general and specific plans as specified in Section 65451;

The proposed subdivision map is consistent with the goals, objectives, policies, and land use designations of the applicable General Plan and any relevant Specific Plans adopted for the area. The project complies with all applicable land use, circulation, and development standards and furthers the intent of the City's planning documents. Therefore, this finding cannot be made.

That the design or improvement of the proposed subdivision is not consistent with applicable general and specific plans;

The design and proposed improvements of the subdivision, including lot configuration, access, and infrastructure, conform with applicable General and Specific Plan policies and standards. Improvements are consistent with the planned infrastructure network and development pattern anticipated in the area. Therefore, this finding cannot be made.

That the site is not physically suitable for the type of development;

The site is physically suitable for the type of development proposed. The topography, existing conditions, and lack of physical constraints such as flood zones or geologic hazards allow for safe and feasible development. Necessary utilities and infrastructure

can be provided to support the proposed use. Therefore, this finding cannot be made.

That the site is not physically suitable for the proposed density of development;

The proposed density of development is consistent with zoning regulations and the land use designation for the site. The site can adequately accommodate the proposed number of lots or units without resulting in overcrowding or unsafe conditions. Adequate access, open space, and public services are available to support the proposed density. Therefore, this finding cannot be made.

That the design of the subdivision or the proposed improvements are likely to cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat;

The design of the subdivision and associated improvements will not cause substantial environmental damage or significantly and avoidably impact fish or wildlife habitat in that there are no environmentally sensitive areas within or around the site. Therefore, this finding cannot be made.

That the design of the subdivision or type of improvements is likely to cause serious public health problems; and

The design of the subdivision and proposed improvements will not result in any serious public health problems. The project will comply with all applicable building codes, health regulations, and safety standards. Required public services, including fire protection, water, and wastewater infrastructure, are available and sufficient to serve the development. Therefore, this finding cannot be made.

That the design of the subdivision or the type of improvements will conflict with easements, acquired by the public at large, for access through or use of, property within the proposed subdivision. In this connection, the governing body may approve a map if it finds that alternate easements, for access or for use, will be provided, and that these will be substantially equivalent to ones previously acquired by the public. This subsection shall apply only to easements of record or to easements established by judgment of a court of competent jurisdiction and no authority is hereby granted to a legislative body to determine that the public at large has acquired easements for access through or use of property within the proposed subdivision."

The subdivision does not conflict with any public easements for access or use. All existing public easements of record have been identified and appropriately preserved, or alternate easements of substantially equivalent utility will be provided where necessary. The proposed map ensures continued public access and utility connectivity as required. Therefore, this finding cannot be made.

Pursuant to the proposed subdivision design, none of the above findings for denial can be made or associated with the proposed project.

Design Review Findings - Costa Mesa Municipal Code Section 13-29(g)(14)

The project complies with the City of Costa Mesa Zoning Code and meets the purpose and intent of the residential design guidelines, which are intended to promote design excellence in new residential construction, with consideration being given to compatibility with the established residential community. The design review includes site planning, preservation of overall open space, landscaping, appearance, mass and scale of structures, location of windows, varied roof forms and roof plane breaks, and any other applicable design features.

The project proposes a maximum density of one dwelling unit per 3,630 square feet of lot area, which is consistent with the maximum allowable density for the R2-MD Zone and the Medium Density Residential land use designation. The project complies with all other applicable Zoning Code standards including setbacks, parking, and open space. The project design incorporates elevations with varied roof forms and projections including eaves and overhangs to provide visual interest as viewed from the street. The exterior materials include brick veneer and horizontal wood siding, white stucco and composite shingle roofing. The architecture of the homes includes varying roof forms, wall planes, and exterior materials which allow it to avoid a boxy two-story design. Landscaping throughout the project meets code requirements and the project would introduce 22 new trees on-site. 11 of which are palms. The project will not result in privacy impacts to the surrounding residences based on the proposed window fenestration patterns and the proposed setbacks from the neighboring properties.

The visual prominence associated with the construction of a two-story house or addition in a predominantly single-story neighborhood has been reduced through appropriate transitions between the first and second floors and the provision of second floor offsets to avoid unrelieved two-story walls.

The neighborhood includes a mixture of one and two-story residential properties consisting of apartments, duplexes, condos, and single-family residences. The abutting property to the northeast consists of an apartment complex and the two abutting properties to the southeast consist of detached single-family residences. The second story design proposes a second-floor area that will not exceed the floor area of the first floor which complies with the design guidelines.

As applicable to affordable multi-family housing developments, the project complies with the maximum density standards allowed pursuant to the general plan and provides affordable housing to low or very low-income households, as defined

by the California Department of Housing and Community Development. The project includes long-term affordability covenants in compliance with State law.

The application does not include an affordable multi-housing development component and therefore, the requirement to include an affordability covenant is not applicable to the project.

ENVIRONMENTAL DETERMINATION

The project is categorically exempt from the provisions of the California Environmental Quality Act (CEQA) pursuant to CEQA Guidelines Section 15315 (Class 15), Minor Land Divisions for the parcel map application, and Section 15303 (Class 3), New Construction or Conversion of Small Structures for the design review application. In addition, this project is statutorily exempt from the requirements of CEQA pursuant to Public Resources Code section 21080.66

Under Class 15, the division of property in urbanized areas is exempt from the provisions of CEQA if the subdivision: is zoned for residential use, is being subdivided into four or fewer parcels, conforms with the General Plan and Zoning Code, is accessible and serviceable by utilities, was not involved in a division of a larger parcel within the previous two years, and has an average slope less than 20 percent. The proposed project meets the aforementioned conditions as described under CEQA Section 15315 in that:

- The project is located within the City of Costa Mesa and is consistent with the R2-MD Zoning Designation and the Medium Density Residential General Plan Designation because it proposes lot areas and development in compliance with zoning requirements, and proposes a density allowed by the General Plan Land Use Density;
- The project site is serviceable by all utilities and is accessible to the public right of way;
- The parcel has not been involved in a previous subdivision in the previous two years; and
- The parcel has been previously graded, is relatively flat and has an average slope less than 20 percent.

Additionally, under Section 15303 (Class 3), a project is exempt from the provisions of CEQA if it includes the construction or conversion of less than three new single-family residences in an urbanized area. The proposed project meets this requirement in that it includes the replacement of two existing single-family residences with two new single-family residences in an urban area. Lastly, none of the exceptions that bar the application of a categorical exemption pursuant to CEQA Guidelines Section 15300.2 applies. Specifically, the project would not result in a cumulative environmental impact; would not have a significant effect on the environment due to unusual circumstances;

would not result in damage to scenic resources; is not located on a hazardous site or location; and would not impact any historic resources.

ALTERNATIVES

As an alternative to the recommended action, the Planning Commission may:

<u>Approve the project with modifications</u>. The Planning Commission may suggest specific changes that are necessary to alleviate concerns. If any of the additional requested changes are substantial, the item should be continued to a future meeting to allow a redesign or additional analysis. In the event of significant modifications to the proposal, staff will return with a revised Resolution incorporating new findings and/or conditions.

<u>Deny the project</u>. If the Planning Commission believes that there are insufficient facts to support the findings for approval, the Planning Commission must deny the application, provide facts in support of denial, and direct staff to incorporate the findings into a Resolution for denial. If the project is denied, the applicant could not submit substantially the same type of application for six months. However, because this project is subject to the Housing Accountability Act (Government Code Section 65589.5), if the Planning Commission denies the housing project, and the development is determined to be consistent with applicable, objective general plan, zoning, and subdivision standards and criteria, including design review standards, the Planning Commission must make the following written findings:

- The housing development project would have a specific, adverse impact upon the public health or safety unless the project is disapproved or approved upon the condition that the project be developed at a lower density; and
- There is no feasible method to satisfactorily mitigate or avoid the adverse impact, other than the disapproval of the housing development project or the approval of the project upon the condition that it be developed at a lower density. (Feasible means capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, and technological factors.)

LEGAL REVIEW

The draft Resolution and this report have been approved as to form by the City Attorney's Office.

PUBLIC NOTICE

Pursuant to CMMC Section 13-29(d) three types of public notification have been completed no less than 10 days prior to the date of the public hearing:

- Mailed notice. A public notice was mailed to all property owners and occupants within a 500-foot radius of the project site on July 30, 2025. The required notice radius is measured from the external boundaries of the property.
- 2. **On-site posting.** A public notice was posted on each street frontage of the project site on July 31, 2025.
- 3. **Newspaper publication.** A public notice was published once in the Daily Pilot newspaper on August 1, 2025.

As of the completion date of this report, no written public comments have been received. Any public comments received prior to the August 11, 2025, Planning Commission meeting will be provided separately.

CONCLUSION

Staff supports the applicant's request to develop a Residential Small Lot Subdivision because the proposed development is consistent with the City's R2-MD zoning designation and the General Plan. The project would provide homeownership opportunities to improve the balance between rental and ownership housing in the City. In addition, the proposed development is in compliance with the City's Subdivision and Development Review standards and complies with the City's Residential Design Guidelines. Lastly, the State's Housing Accountability Act (Government Code Section 65589.5) applies to this project and generally requires that cities approve housing projects that are consistent with the General Plan and zoning, unless the City can make specific State mandated findings for denial (further discussed in the "Alternatives" section of this report).